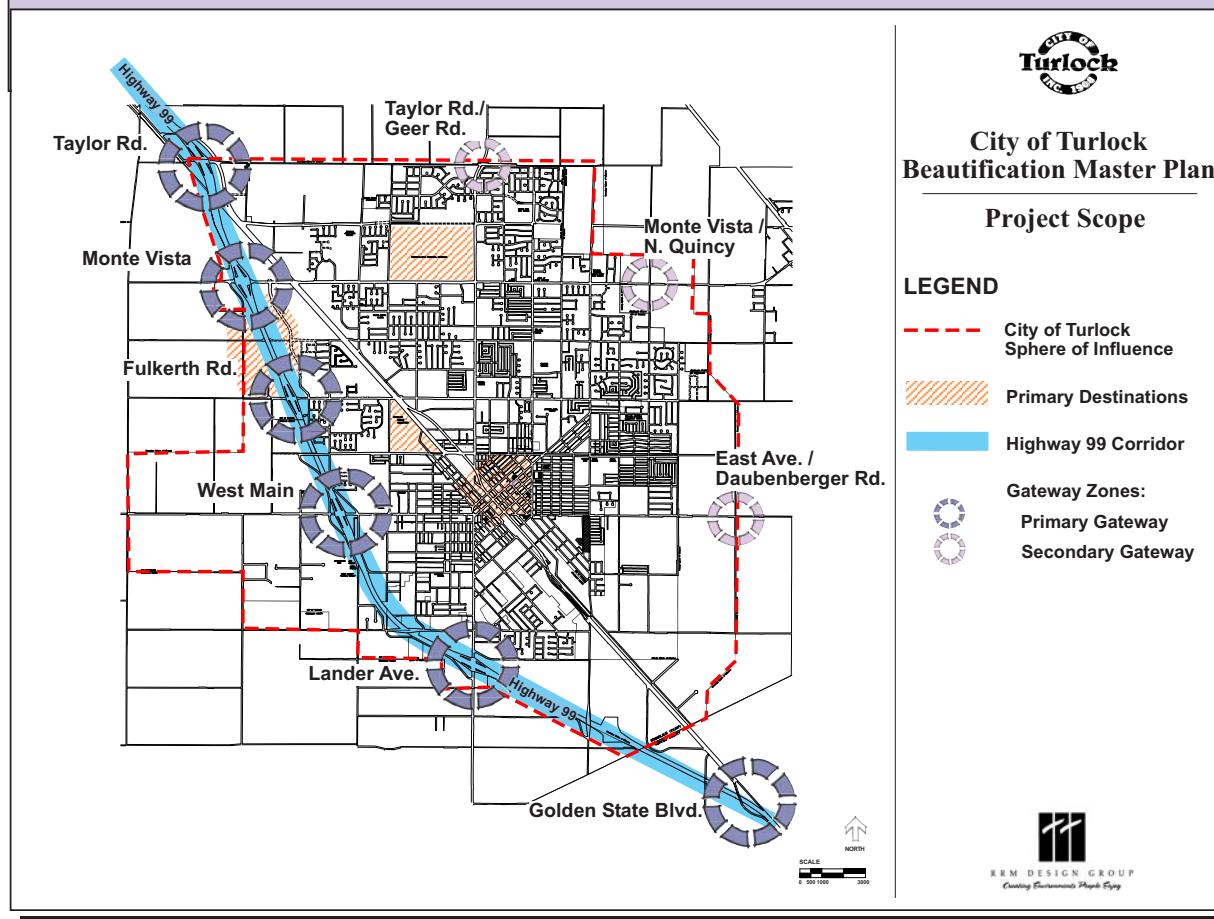


V. GATEWAY ZONES

GATEWAY ZONES GATEWAYS



City of Turlock Beautification Master Plan

Project Scope

LEGEND

- City of Turlock Sphere of Influence
- Primary Destinations
- Highway 99 Corridor
- Gateway Zones:
Primary Gateway
Secondary Gateway



OBJECTIVES

Gateway Zones encompass the entire entry experience and include primary gateways, secondary gateways, and the gateway corridors. Gateways are entrances into the City of Turlock, and gateway corridors are the defined travel routes that extend from these entrances into primary destination areas of the City.

GATEWAYS

Gateways are intended to identify primary and secondary entrances into the City of Turlock. Incorporation of "gateway scenes" at the defined gateways will function as a visual anchor to clearly identify the area as a gateway. "Gateway scenes" should be composed of specific elements that serve to provide a sense of arrival, reaffirm direction, and reinforce the identity and character of Turlock. Primary and secondary gateways are defined as the following:

Primary

- Hwy 99/ Taylor Rd.
- Hwy 99/ Monte Vista Ave.
- Hwy 99/ Fulkerth Rd.
- Hwy 99/ West Main St.
- Hwy 99/ Lander Ave.
- Hwy 99/ Golden State Blvd.

Secondary

- Geer Rd./ Taylor Rd.
- Monte Vista Ave./ N. Quincy Rd.
- East Ave./ Daubenberger Rd.

Objectives:

- ↳ Identify as a Major Gateway
- ↳ Announce Entry into Turlock
- ↳ Reaffirm Direction
- ↳ Visually Enhance the Gateway
- ↳ Reinforce City Identity
- ↳ Reflect the Character of the Gateway

GATEWAY ZONES

GATEWAYS

P ROPOSED ELEMENTS

SIGNAGE



Gateway Monumentation

Gateway Monumentation should be incorporated into the design of the “gateway scene” at both the primary and secondary gateways. Gateway signage will need to be addressed in greater detail in a signage plan and coordinated with Caltrans for incorporation into the primary gateways. The design for the monuments may vary at each gateway, but should use the same design vocabulary, color scheme, and graphics. The monuments should be reflective of the character of the gateway, appropriately scaled, and legible to motorists.

STREETSCAPE



Fencing

Fencing should be an integral part of the “gateway scene” for both primary and secondary gateways. Fencing will help to define the gateways, providing a backdrop that reflects the design vocabulary and reinforces Turlock’s identity.

Primary Gateway: Proposed fence design for the primary gateways may vary but should incorporate forms, materials, and colors associated with the typical post and wire fencing that dominates Turlock’s countryside.

Secondary Gateway: Proposed fence design for the secondary gateways may vary but should be rural in character and incorporate forms, materials, and colors associated with painted post and rail fences.



Brick Pavers

Brick masonry and paving is an element that is visually associated with the downtown historic buildings and urban core. Incorporation of brick pavers in the design for the primary gateways at

the Highway 99 / West Main Street and Highway 99 / Lander Avenue interchange will reinforce their connection to downtown and help orient the traveler.

LANDSCAPE



“Orchard” Planting

Landscape provides an opportunity to reinforce Turlock’s connection to the agrarian countryside and to enhance the City’s gateways. Variations of the “orchard” planting will extend the concept introduced along the Highway 99 corridor. Plant material, color, and forms should be used to reinforce the unique character of each gateway. Primary, urban gateways should be more structured and formal. Secondary gateways should be less formal.



Seasonal Interest Plants

Plant material proposed for the primary and secondary gateways should be selected to reflect seasonal change. Ornamental shrubs, annuals, and perennials should be introduced to emphasize entry, introduce the character of the corridor, and reinforce Turlock’s agricultural character.

PUBLIC ART



Public Art

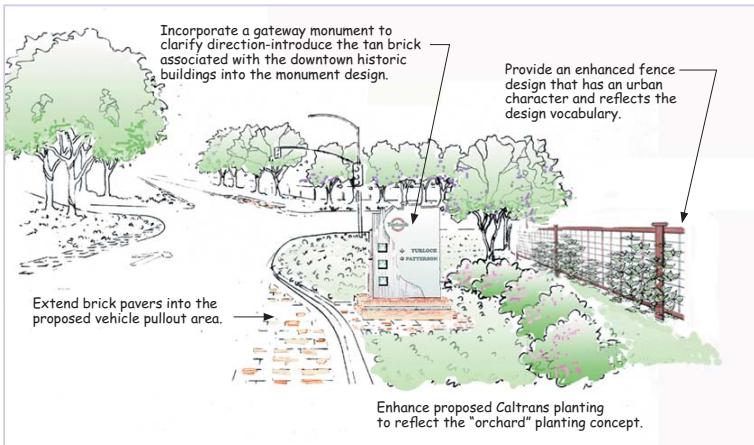
Gateways are the front door to the City and are prime locations for public art because of the visual exposure they provide. Public art helps to define gateways by providing an image that travelers associate with. Public art should be incorporated into the primary gateways either as part of the design for the gateway monuments or in separate pieces incorporated into the “gateway scene”.

Gateways should announce entry into Turlock by incorporating specific elements that create a “gateway scene”. The composition should function as a visual anchor and provide a sense of arrival, reaffirm direction, and reinforce the identity of Turlock. The specific elements proposed for the “gateway scene” should reflect the history and spirit of Turlock and include gateway monumentation, fencing, “orchard” planting, and seasonal interest plants. The design for each of the defined gateways may vary, although the elements comprising the “gateway scene” should be repeated to reinforce the concept of entry. The narrative on the left side of this page further defines the elements proposed for the primary and secondary gateways and discusses how they should be incorporated into the “gateway scene”. The following pages provide further design solutions for some of the key gateways.

GATEWAY ZONES GATEWAYS



Existing



Proposed

C ONCEPTUAL DESIGN SOLUTION

C HIGHWAY 99 / WEST MAIN STREET INTERCHANGE

The City should work with Caltrans to incorporate the proposed landscape improvements for the West Main Street interchange into a gateway design solution. The proposed improvements should be incorporated into a “gateway scene” that includes gateway monumentation, fencing, “orchard” planting, and seasonal interest plants. The connection to the downtown should be reinforced by introducing tan brick into the design of the gateway monument. This will reflect the brick found in many of the historic buildings downtown. Brick pavers used as accent in the proposed vehicle pullout area will also reinforce the downtown connection and help orient the traveler. Incorporation of public art into the “gateway scene” will provide an opportunity to expand on current art projects and themes proposed for the downtown core.

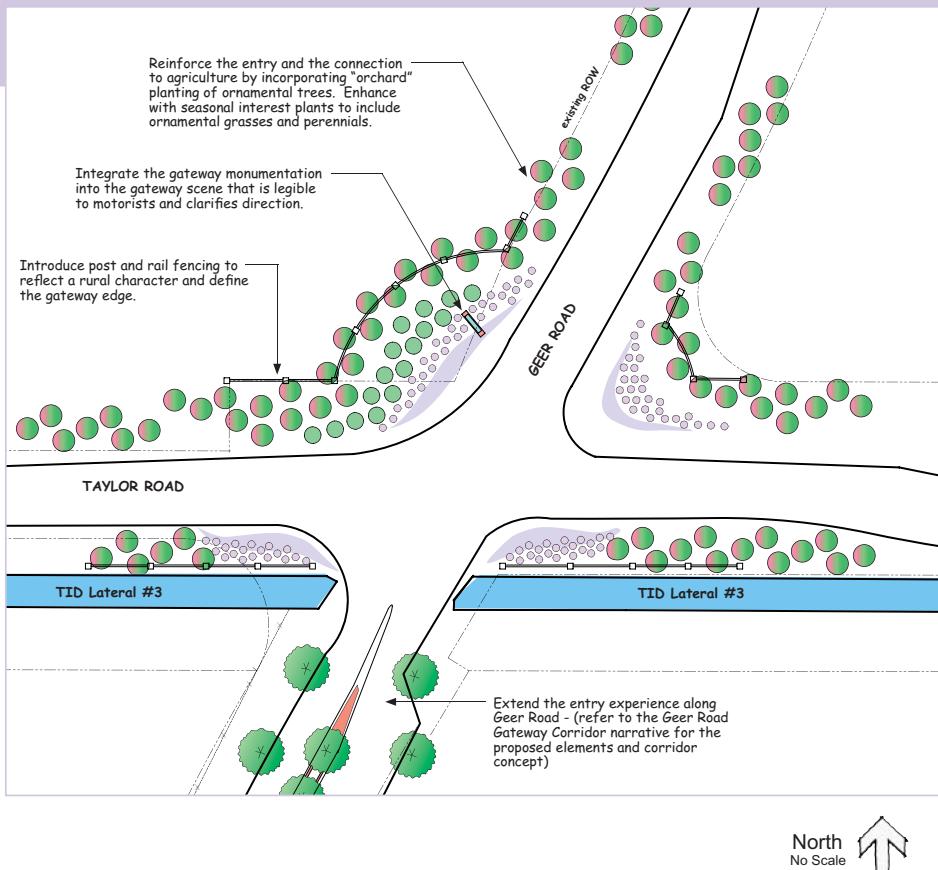
Goals

- ❖ Identify as a Major Gateway
- ❖ Announce Entry into Turlock
- ❖ Reaffirm Direction
- ❖ Visually Enhance the Gateway
- ❖ Reinforce City Identity
- ❖ Reflect the Character of the Gateway

Implementation

- ❖ Capitalize on the proposed Caltrans design by incorporating the proposed improvements into the gateway design.
- ❖ Provide a gateway scene - associate gateways with specific design elements to include gateway monumentation, fencing, “orchard” planting, and seasonal interest plants.
- ❖ Reinforce connection to the downtown by incorporating tan brick, brick pavers, a refined fence design, and Public Art.

GATEWAY ZONES GATEWAYS



C ONCEPTUAL DESIGN SOLUTION

GEER ROAD / TAYLOR ROAD INTERSECTION

The gateway proposed for the Geer Road and Taylor Road intersection will define the entry into Turlock for motorists traveling south along County Route J14. This conceptual design solution relies on expanding the landscape and monumentation improvements to the north side of Taylor Road which is outside of the City limits. This is an important gateway, and as such, the City should negotiate with the County and adjacent land owners to acquire the necessary easements to develop the expanded design to the extent practicable. If this is not possible, then the final design will need to focus on establishing a high impact design within the limited area on the south side of Taylor Road. The proposed elements should reinforce the rural character of this northern gateway and help to define entry and clarify direction.

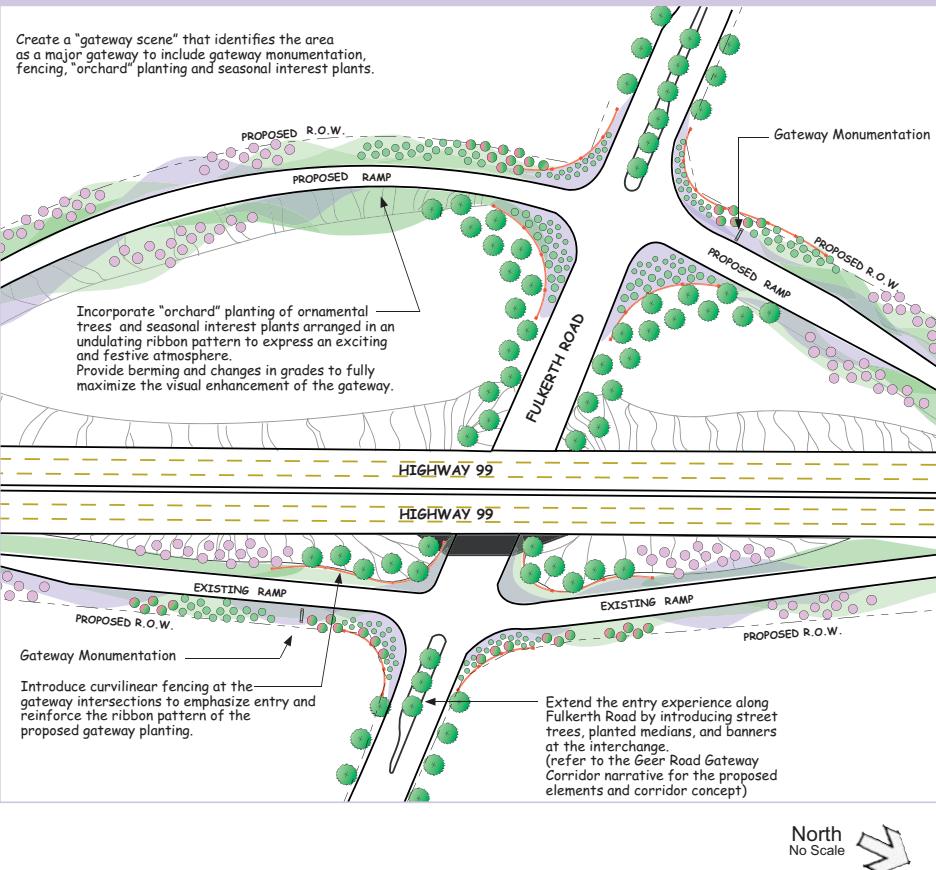
Goals

- ❖ Identify as a Secondary Gateway
- ❖ Announce Entry into Turlock
- ❖ Provide Sense of Arrival
- ❖ Reaffirm Direction

Implementation

- ❖ Create a rural "gateway scene" to include gateway monumentation fencing, "orchard" planting, and seasonal interest plants.

GATEWAY ZONES GATEWAYS



C CONCEPTUAL DESIGN SOLUTION

HIGHWAY 99 / FULKERTH ROAD INTERCHANGE

The primary gateway proposed for the future Fulkерth Road interchange should announce entry into the City of Turlock and express a festive and exciting atmosphere to promote the Regional Commercial Centers Area and the Stanislaus County Fairgrounds. The proposed "gateway scene" should be significant in scale and include gateway monumentation, fencing, "orchard" planting, and seasonal interest plants. "Orchard" planting and seasonal interest plants should be arranged to reflect ribbons of color. The existing topography should be enhanced and berms should be introduced to maximize the ribbon appearance of the planting. The proposed fencing should incorporate the design vocabulary and be curvilinear in form to reinforce the gateway's festive atmosphere.

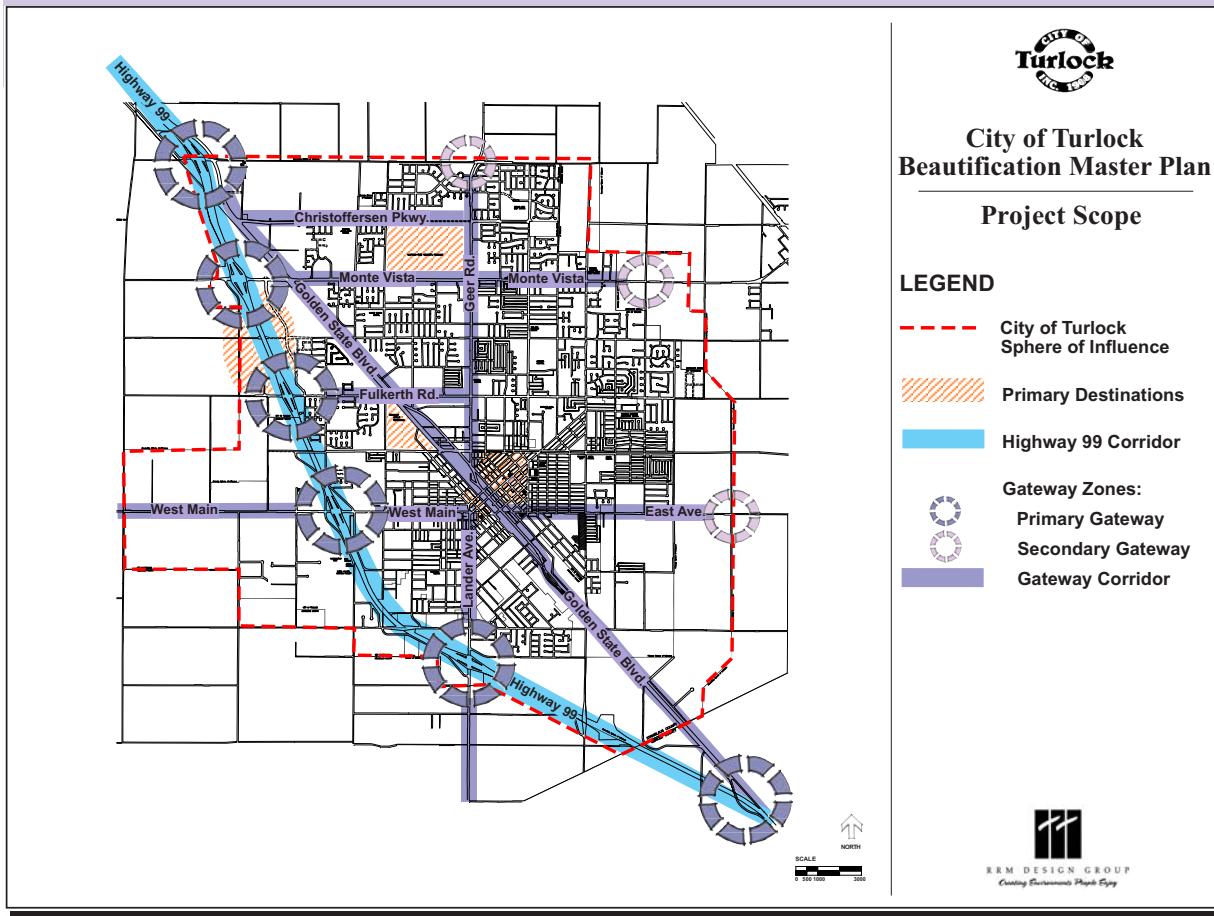
Goals

- ❖ Identify as a Major Gateway
- ❖ Announce Entry into Turlock
- ❖ Reaffirm Direction
- ❖ Visually Enhance the Gateway
- ❖ Reinforce City Identity
- ❖ Reflect the Character of the Gateway

Implementation

- ❖ Create a gateway scene - associate gateways with specific design elements to include gateway monumentation, fencing, "orchard" planting, and seasonal interest plants.
- ❖ Reinforce the connection to the Regional Commercial Centers Area and the Stanislaus County Fairgrounds through expressive planting, grading, and fence design.

GATEWAY ZONES GATEWAY CORRIDORS



City of Turlock Beautification Master Plan

Project Scope

LEGEND

- Dashed red line: City of Turlock Sphere of Influence
- Orange hatched area: Primary Destinations
- Blue line: Highway 99 Corridor
- Purple circle: Gateway Zones: Primary Gateway, Secondary Gateway, Gateway Corridor

OBJECTIVES

Gateway corridors are important because they provide an opportunity to extend the entry experience. Repetition of the design vocabulary, used in the architectural and landscape elements proposed for the gateways, will provide visual recognition, clarify direction, and reinforce a unified City identity. The following are defined as gateway corridors:

Golden State Boulevard
Christoffersen Parkway
Monte Vista Avenue
Fulkerton Road
West Main Street
East Avenue
Geer Road
Lander Avenue

Objectives:

- Extend the Arrival Experience
- Enhance the Character of the Corridor
- Reinforce the Identity of Turlock
- Clarify Direction
- Provide a Sense of Hierarchy
- Mitigate Negative Visual Impacts

GATEWAY ZONES

GATEWAY CORRIDORS

SIGNAGE



Directional Monumentation

Directional Monumentation should be incorporated into the key intersections identified in the narrative for each of the gateway corridors. The monumentation should be responsive to the overall design of the corridor and legible to motorists.

Additional emphasis should be given to the key intersections that function as entries to Turlock's downtown core. The design for the entries should be coordinated with the Downtown Turlock Zoning Districts and Design Guidelines Document.



Roadway Approach Signs

Roadway approach signs should be located to notify motorists of the upcoming intersection so that they may turn in a timely manner. Approach signs should be located prior to the key intersection identified in the narrative for each of the gateway corridors.



Street Signs

Street signs provide a recognizable sense of repetition, and are necessary in defining the gateway corridors. Street signs should be incorporated into the intersections of all identified gateway corridors.



Banners

Banners should be integrated into the design of the gateway corridors to publicize special events, express the character of the corridor, or add color and excitement. The banners should be introduced into the design of gateway corridors.

STREETSCAPE



Medians

Medians should be incorporated into the gateway corridors where possible, and existing medians should be enhanced to extend the arrival experience. The proposed medians should be designed to reflect the character of the corridor.



Brick Pavers

Brick masonry and paving is an element that is visually associated with the downtown historic buildings and urban core. Brick pavers should be used as an accent in the design for the sidewalks that parallel the corridors to create a pedestrian atmosphere.



Crosswalks

Brick enhanced crosswalks should be a part of the design for pedestrian oriented gateway corridors. Enhanced crosswalks should be incorporated into the key intersections of gateway corridors.

In addition, key intersections on Golden State Boulevard associated with the downtown should include brick enhanced crosswalks.



Bus Shelters

The Bus Line Service of Turlock is currently rerouting bus routes to better accommodate passengers and will provide stops along all of the defined gateway corridors. As this new service is established, bus shelters located along these corridors will promote a pedestrian friendly environment, provide a strong unifying element, and serve to encourage ridership.

P ROPOSED ELEMENTS

The architectural and landscape elements proposed for the gateway corridors should visually extend the entry experience. Repetition of the design vocabulary used in the elements proposed for the gateways will reinforce Turlock's identity, visually unify the City, and provide travelers with the visual cues necessary to navigate the City's gateways and corridors. The proposed signage for the gateway corridors will provide visual continuity and clarify direction. The specific design for the signs and location will need to be addressed in greater detail in a comprehensive signage plan. The streetscape elements proposed for the gateway corridors will establish a hierarchy for Turlock's street network and will visually unify the corridors. The elements will provide a framework for future design projects and ensure a cohesive, unified character that is reflective of Turlock's identity.

The narrative to the left and on the following page further defines the proposed elements and discusses how they should be incorporated into the design of the gateway corridors. The balance of this section includes conceptual design solutions for a variety of sites along the designated gateway corridors.

GATEWAY ZONES GATEWAY CORRIDORS

PROPOSED ELEMENTS



Planters

Planters should be located on commercial corridors to visually unify the corridor and provide a pedestrian scale. They create interest, and an opportunity to incorporate planting and materials reflective of the character of the corridor.



Planter Urns

Planter urns provide a place for seasonal planting of annual and perennial color and reinforce a pedestrian, urban environment. Urns should be incorporated into key intersections that define entry to Turlock's downtown core.



Light Fixtures

Historic light fixtures should be incorporated into the gateway corridors that lead to the downtown to create a pedestrian friendly environment and reinforce the connection to downtown. The fixtures should reflect the historic character of the older, more established corridors. New fixtures should be introduced and existing fixtures supplemented along gateway corridors. In addition, consolidation of street signage and light fixtures will minimize visual clutter.

LANDSCAPE



Street Trees

Street trees provide visual continuity and should be an integral part of the design for the gateway corridors. Trees should be located on both sides of the corridor and in proposed and existing medians where possible. Proposed trees should be defined in the Street Tree Master Plan and should reflect the character of the corridor.



Seasonal Interest Plants

Plants with seasonal interest should be used to accent identified key intersections along the gateway corridors. Ornamental shrubs, annuals and perennials should emphasize seasonal change, and enhance the character of the corridor.

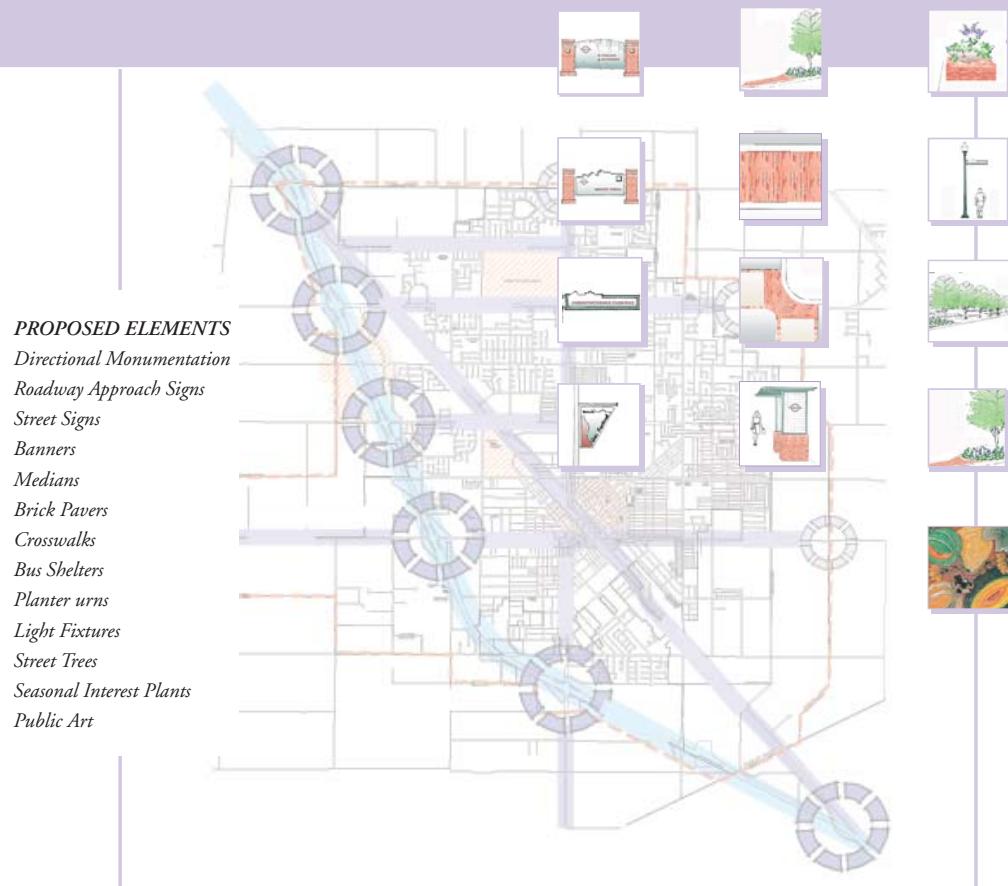


PUBLIC ART

Public Art

Public art provides an opportunity to express the unique character of the corridor. Introduction of smaller scale public art projects in the identified key intersections along the gateway corridors will ensure exposure, define the character of the corridor and provide unique visual markers. The recently approved public art project proposed for the intersection of Lander Avenue and West Main Street is an ideal location and will provide a visual anchor for the downtown core.

GATEWAY ZONES GATEWAY CORRIDORS



OLDEN STATE BOULEVARD

Golden State Boulevard is an expressway designed to accommodate significant portions of interregional travel. The defined gateway corridor encompasses the length of the boulevard from Taylor Road on the north, to the Highway 99 interchange on the south. A portion of the corridor, defined as the downtown section, extends through the center of downtown, and is urban in character. The remainder of the corridor, to the north and south of downtown, is industrial and rural. The design elements proposed for the corridor should reflect this varying character while providing a visually unified travel experience.

The elements proposed for the length of the corridor include street trees, (defined in the Street Tree Master Plan), planted medians, street signs and bus shelters. Proposed street trees should be located along the edges of the corridors and in the medians to unify the corridor. Large-scale conifers introduced into the medians associated with the rural sections of the corridor will reduce the visual scale of the corridor. Directional monumentation, roadway approach signs, seasonal interest plants, and public art incorporated into the identified key intersections will clarify direction and reinforce a hierarchy along the corridor.

The downtown section of Golden State Boulevard includes the Canal Drive intersection and extends south to the industrial development located at the Minerva Street intersection. The additional elements proposed for this section of the corridor will signal transition into a more pedestrian, urban environment and will include roadway approach signs, brick pavers, light fixtures and banners. To emphasize entry into the downtown core, planter urns and enhanced crosswalks should be introduced at key intersections such as Marshall Street, East Main Street, and Olive Avenue. The design for these intersections and the proposed signage should be addressed in greater detail in the signage plan and coordinated with the Downtown Turlock Zoning Districts and Design Guidelines Document.

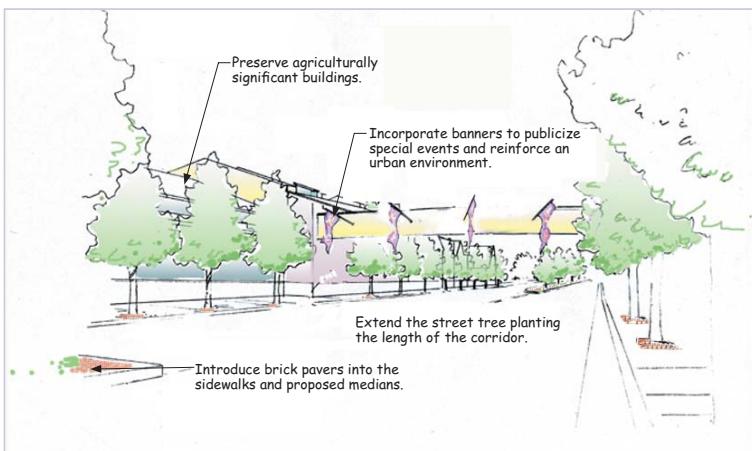
KEY INTERSECTIONS:

- Golden State Blvd. / Christoffersen Pkwy.
- Golden State Blvd. / Monte Vista Ave.
- Golden State Blvd. / Fulkerth Rd.
- Golden State Blvd. / Geer Rd.
- Golden State Blvd. / East Olive Ave.
- Golden State Blvd. / East Main St.
- Golden State Blvd. / Marshall St.
- Golden State Blvd. / Berkeley Ave.

GATEWAY ZONES GATEWAY CORRIDORS



Existing



Proposed

C ONCEPTUAL DESIGN SOLUTION

SOUTH GOLDEN STATE BOULEVARD INDUSTRIAL GATEWAY

Incorporate design elements that create an industrial gateway and signal transition into downtown. Preserve the rich agricultural heritage of Turlock by investing in the preservation and restoration of culturally and historically significant buildings and structures. Introduce light fixtures, banners, and brick pavers into the medians and sidewalks, in addition to the elements proposed for the length of the corridor, to highlight the more urban environment of downtown.

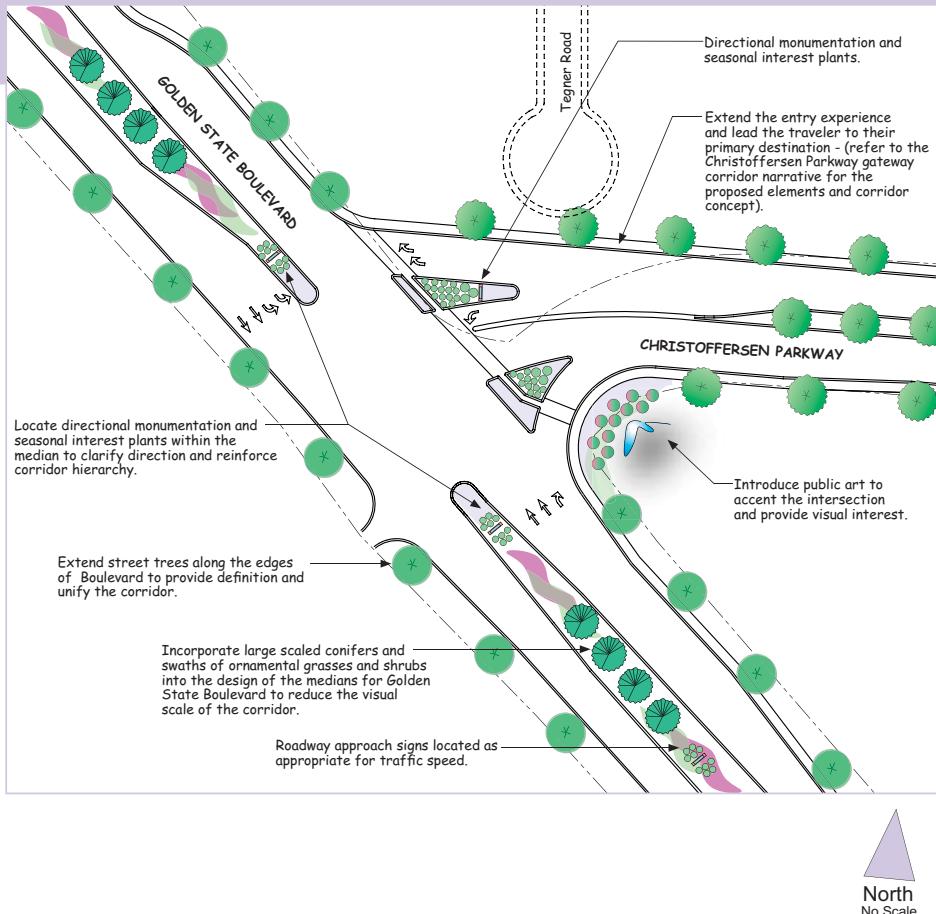
Goals

- ↪ Extend the Arrival Experience
- ↪ Enhance the Character of the Corridor
- ↪ Reinforce the Identity of Turlock
- ↪ Clarify Direction
- ↪ Provide a Sense of Hierarchy
- ↪ Minimize Negative Visual Impacts

Implementation

- ↪ Preserve culturally significant buildings and structures.
- ↪ Incorporate elements, in addition to those proposed for the length of the corridor, to transition into the urban environment.

GATEWAY ZONES GATEWAY CORRIDORS



C CONCEPTUAL DESIGN SOLUTION

GOLDEN STATE BOULEVARD AND CHRISTOFFERSEN PARKWAY INTERSECTION

The Christoffersen Parkway and Golden State Boulevard intersection is identified as a key intersection and is important because both corridors are expressways designed to accommodate significant amounts of traffic loads. The elements proposed for the intersection should emphasize the intersection, clarify direction, and provide the visual cues and information necessary to identify potential travel routes. It should include directional monumentation, seasonal interest plants, and public art. Roadway approach signs should be located prior to the intersection allowing motorists to change lanes and turn in a safe manner. Public art incorporated into the intersection will provide exposure and visual interest, and will establish an image travelers will remember.

Goals

- ❖ Extend the Arrival Experience
- ❖ Enhance the Character of the Corridor
- ❖ Reinforce the Identity of Turlock
- ❖ Clarify Direction
- ❖ Provide a Sense of Hierarchy
- ❖ Minimize Negative Visual Impacts

Implementation

- ❖ Extend the entry experience by incorporating street trees, planted medians, street signs, and bus shelters.
- ❖ Emphasize the intersection by introducing additional elements to include roadway approach signs, directional monumentation, seasonal interest plants, and public art.
- ❖ Reduce the visual scale of the corridor by incorporating conifers and ornamental grasses into the median design for the north and south sections of Golden State .

GATEWAY ZONES GATEWAY CORRIDORS

- PROPOSED ELEMENTS**
- Directional Monumentation
 - Roadway Approach Signs
 - Street Signs
 - Medians
 - Bus Shelters
 - Light Fixtures
 - Street Trees
 - Seasonal Interest Plants
 - Public Art



CHRISTOFFERSEN PARKWAY

Christoffersen Parkway is an expressway designed to accommodate traffic loads generated by future development in north Turlock and to alleviate City traffic on Taylor Road. The gateway corridor is defined as the portion of Christoffersen Parkway extending from the Golden State Boulevard intersection, east to the Geer Road Intersection. The proposed corridor elements should emphasize a parkway environment.

The elements proposed for the length of the corridor include street trees, planted medians, street signs, and bus shelters. Street trees located along the edges of the corridor and within the proposed medians will create a parkway setting. The tree species proposed for the median planting has been identified in a design and grant application submitted by the City staff, and should be incorporated into the corridor design. Tree species proposed for the edges of the corridor should be coordinated with the median design to unify the corridor.

Directional monumentation, roadway approach signs, seasonal interest plants, and public art incorporated into the identified key intersections will clarify direction and reinforce corridor hierarchy. The key intersection at Golden State Boulevard should be emphasized. A conceptual design solution provides direction for incorporation of the proposed elements within the intersection. The design and location of the directional monumentation and roadway approach signs should be addressed in greater detail in a sign plan.

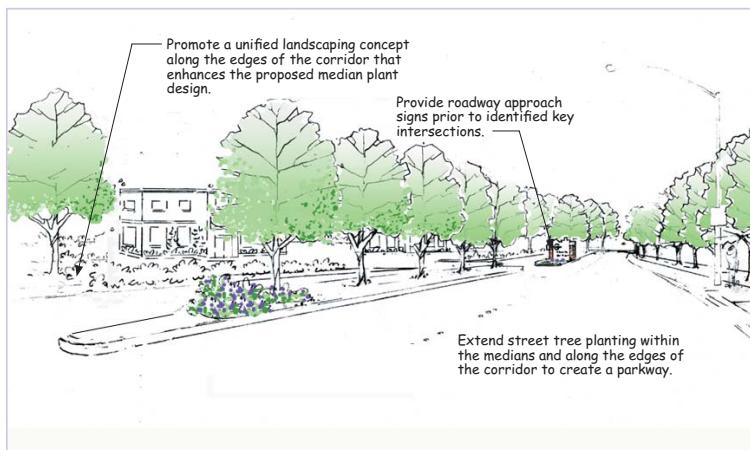
KEY INTERSECTIONS

- Christoffersen Pkwy.. / Golden State Blvd.
- Christoffersen Pkwy.. / Geer Rd.

GATEWAY ZONES GATEWAY CORRIDORS



Existing



Proposed

C CONCEPTUAL DESIGN SOLUTION

CHRISTOFFERSEN PARKWAY

Create a parkway setting by providing a canopy of shade. Introduce street trees, planted medians, street signs and bus shelters along the corridor. Refer to the design and grant application submitted by the City for specified tree species in the median. Unify the corridor with a planting concept for the edges of the corridor that reflects the median planting design. Incorporate the use of reclaimed water for irrigation.

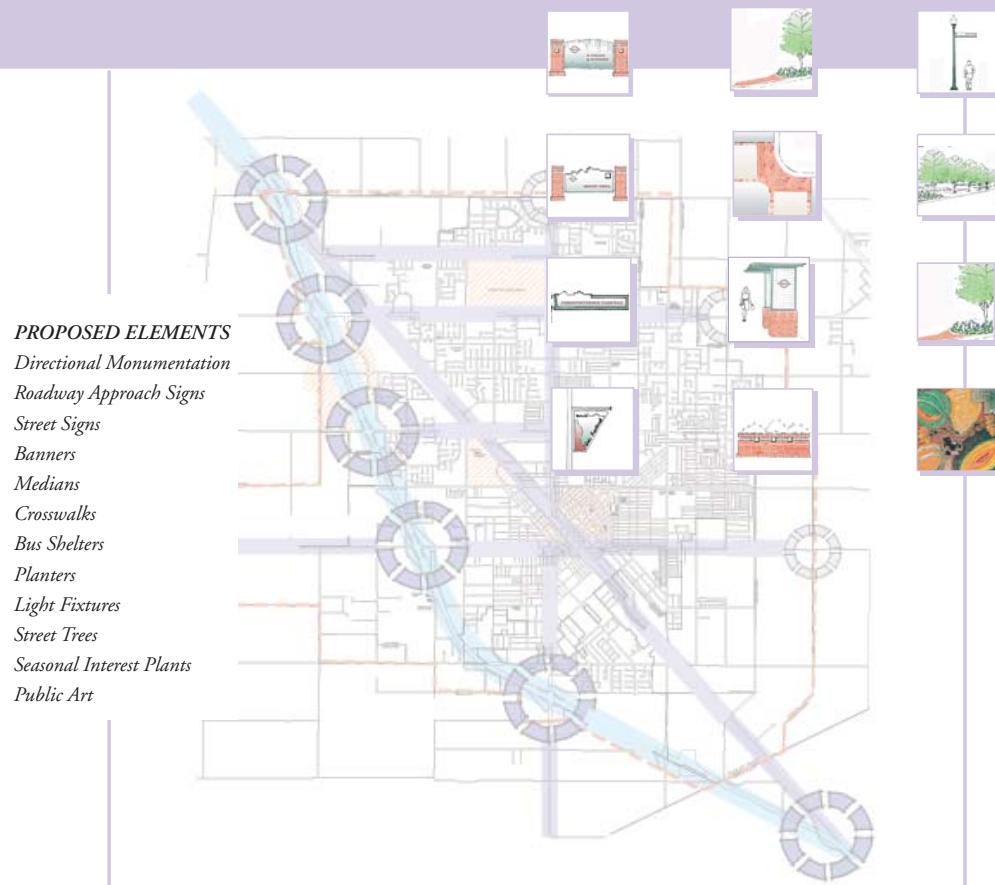
Goals

- ❖ Extend the Arrival Experience
- ❖ Enhance the Character of the Corridor
- ❖ Reinforce the Identity of Turlock
- ❖ Clarify Direction
- ❖ Provide a Sense of Hierarchy
- ❖ Minimize Negative Visual Impacts

Implementation

- ❖ Introduce street trees in the medians and extend the concept into the corridor edges to provide a parkway atmosphere.
- ❖ Incorporate street signs and bus shelters.
- ❖ Introduce directional monumentation, roadway approach signs, seasonal interest plants, and public art at key intersections.

GATEWAY ZONES GATEWAY CORRIDORS



MONTE VISTA AVENUE

Monte Vista Avenue is an arterial that is defined as a gateway corridor from Highway 99 to Quincy Road. Because it serves as the main corridor leading to the CSU campus, the proposed elements and design should reinforce the campus environment. The elements proposed for the length of the corridor include street trees, (defined in the Street Tree Master Plan), planted medians, street signs, planters, and bus shelters. The existing median network should be extended and supplemented with street trees and a manicured lawn. The paving proposed for the medians should incorporate an exposed aggregate finish to match the existing medians along the corridor. The medians will visually extend the campus landscape and help to define the character of the corridor.

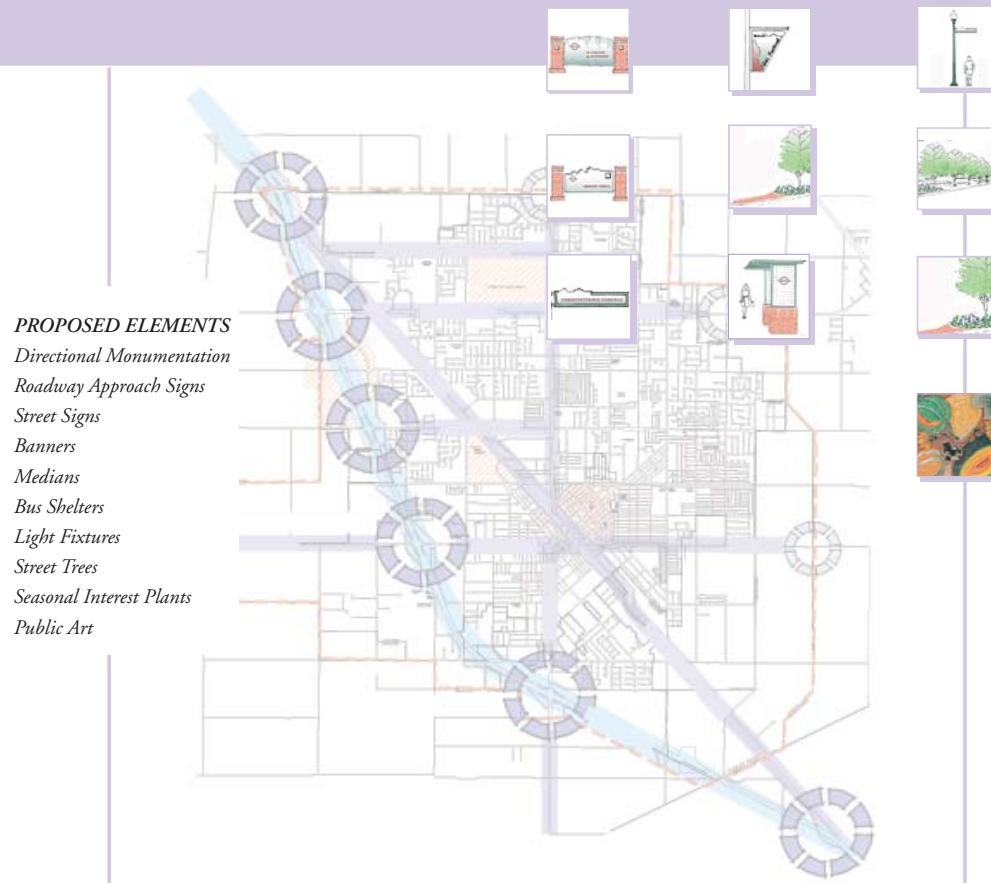
Directional monumentation, roadway approach signs, seasonal interest plants, and public art incorporated into the identified key intersections will clarify direction and reinforce corridor hierarchy. The key intersection at Geer Road defines the entry to the CSU campus and should be designed to emphasize the campus environment and reinforce a pedestrian atmosphere. The segment extending from Geer Road to Crowell Road provides an opportunity to introduce the campus and publicize special events by incorporating enhanced crosswalks and sidewalks, banners, directional monumentation, and other streetscape elements into the design. The location and design of the banners and proposed signage should be addressed in greater detail in the Sign Plan.

KEY INTERSECTIONS

Monte Vista Ave. / Golden State Blvd.

Monte Vista Ave. / Geer Rd.

GATEWAY ZONES GATEWAY CORRIDORS



FULKERTH ROAD

Fulkert is an arterial that is classified as a gateway corridor from Highway 99 to Geer Road. It serves as the primary corridor to the County Fairgrounds and the Regional Commercial Centers Area and should reflect regional character and a festive environment.

The elements proposed for the length of the corridor include street trees, planted medians, street signs, planters, banners, and bus shelters. The existing median network should be extended and incorporate street trees to reduce the scale of the corridor. The City's Street Tree Master Plan should be updated to specifically address this corridor. Banners, provided the length of the corridor, will add color and excitement and announce fairground events. Proposed planters will visually unify the corridor and provide opportunity to introduce planting and materials that reinforce a regional character and a festive atmosphere.

Directional monumentation, roadway approach signs, seasonal interest plants, and public art incorporated into the identified key intersections will clarify direction and reinforce corridor hierarchy. The design and location of the banners and proposed signage should be addressed in greater detail in the Sign Plan.

KEY INTERSECTIONS

Fulkerth Rd. / Golden State Blvd.
Fulkerth Rd. / Geer Rd.

GATEWAY ZONES GATEWAY CORRIDORS

PROPOSED ELEMENTS

- Directional Monumentation*
- Roadway Approach Signs*
- Street Signs*
- Medians*
- Brick Paving*
- Crosswalks*
- Bus Shelters*
- Planters*
- Light Fixtures*
- Street Trees*
- Seasonal Interest Plants*
- Public Art*



WEST MAIN STREET

West Main Street is an arterial that is classified as a gateway corridor from Washington Road to Lander Avenue at the west entry to downtown. The proposed design elements should reinforce the connection to downtown while preserving the residential character of the corridor.

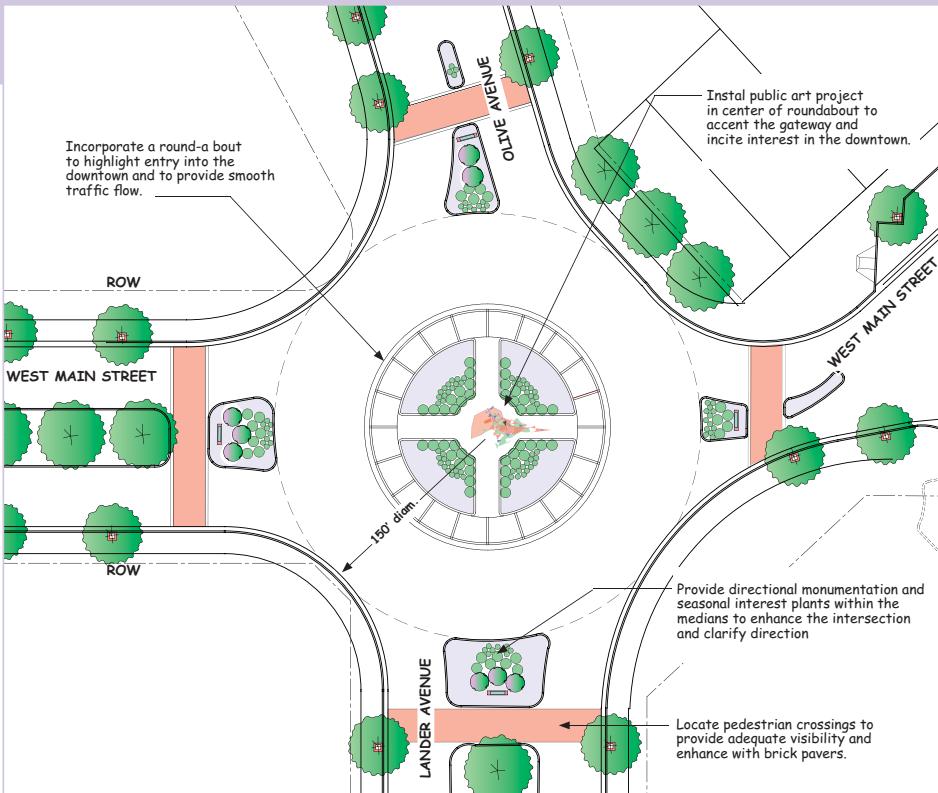
The elements proposed for the length of the corridor include street trees, planted medians, street signs, brick paving, bus shelters, and historic light fixtures. The existing median network should be extended and supplemented. The medians should incorporate brick pavers, and street trees, (defined in the Street Tree Master Plan), to unify the corridor. Existing historic light fixtures should be refurbished or replaced and supplemented to extend the length of the corridor. Sidewalks should be repaired and enhanced with brick pavers to reinforce the pedestrian environment.

The key intersection of West Main and Lander Avenue should be emphasized to define entry into Turlock's downtown core. The intersection should include directional monumentation, roadway approach signs, crosswalks, planter urns, seasonal plants, and public art. The conceptual design solution for this intersection proposes a round-a-bout and suggests locating a public art project in the center of the design. In addition, the proposed signage for this entry should be addressed in greater detail in the Sign Plan and coordinated with the Downtown Turlock Zoning Districts and Design Guidelines Document.

KEY INTERSECTIONS

West Main St. / Lander Ave.

GATEWAY ZONES GATEWAY CORRIDORS



C CONCEPTUAL DESIGN SOLUTION

WEST MAIN STREET AND LANDER AVENUE INTERSECTION

Introduce a round-a-bout to highlight entry into the downtown and provide smooth traffic flow into the City. Incorporate a public art project by incorporating it into the round-a-bout design. Provide directional monumentation, brick paver enhanced crosswalks, planter urns, and seasonal interest plants at the intersections leading up to the round-a-bout to clarify direction, provide hierarchy, and reinforce the connection to the downtown.

Goals

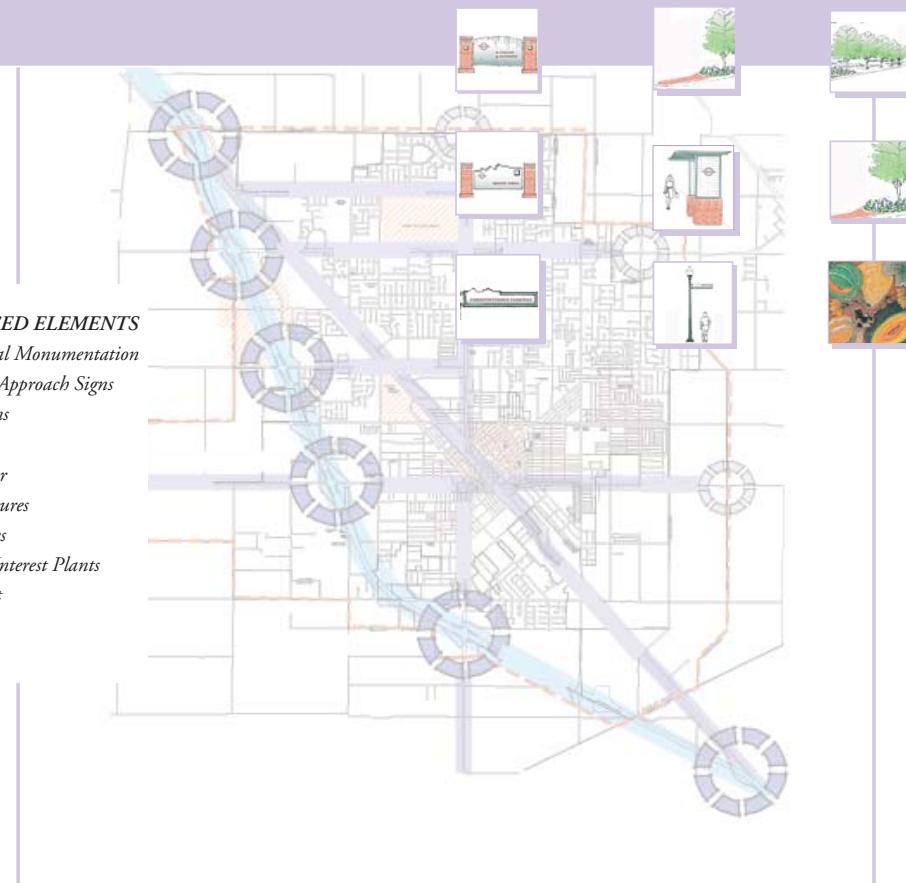
- ❖ Extend the Arrival Experience
- ❖ Enhance the Character of the Corridor
- ❖ Reinforce the Identity of Turlock
- ❖ Clarify Direction
- ❖ Provide a Sense of Hierarchy
- ❖ Minimize Negative Visual Impacts

Implementation

- ❖ Introduce a round-a-bout to reinforce the gateway to downtown.
- ❖ Incorporate elements that define the intersection and reinforce connection to the downtown.
- ❖ Integrate a public art project into the round-a-bout design.

GATEWAY ZONES GATEWAY CORRIDORS

- PROPOSED ELEMENTS**
- Directional Monumentation*
 - Roadway Approach Signs*
 - Street Signs*
 - Medians*
 - Bus Shelter*
 - Light Fixtures*
 - Street Trees*
 - Seasonal Interest Plants*
 - Public Art*



E AST AVENUE

East Avenue is an arterial that is classified as a gateway corridor from the secondary gateway at Daubenerger Road to the heart of Turlock's downtown. The proposed elements for East Avenue should reinforce the connection to downtown while maintaining the rural, residential character of the corridor.

The elements proposed for the length of the corridor include street trees, (defined in the Street Tree Master Plan), planted medians, street signs, bus shelters, and historic light fixtures. Where possible, lane widths should be reduced to provide room for medians enhanced with ornamental trees and seasonal planting. Historic light fixtures should be introduced to reflect the residential character of the corridor and reinforce the connection to downtown.

Directional monumentation, roadway approach signs, seasonal interest plants, and public art incorporated into the identified key intersection will clarify direction and reinforce corridor hierarchy. The design and location of the proposed signage should be addressed in greater detail in a sign plan.

KEY INTERSECTIONS
East Ave. / Golden State Blvd.

GATEWAY ZONES GATEWAY CORRIDORS

- PROPOSED ELEMENTS**
- Directional Monumentation
 - Roadway Approach Signs
 - Street Signs
 - Banners
 - Medians
 - Bus Shelters
 - Brick Pavers
 - Crosswalks
 - Light Fixtures
 - Street Trees
 - Seasonal Interest Plants
 - Public Art



GEER ROAD

Geer Road is an arterial that is classified as a gateway corridor from the secondary gateway at Taylor Road to downtown. The corridor is flanked by strip commercial and office development and is impacted by traffic congestion. The elements proposed for the corridor should reinforce an urban, pedestrian oriented environment and encourage transit use. Foot traffic associated with the CSU Campus and the north Geer Road commercial development should be encouraged. The corridor should also enhance the pedestrian connection between the business development located at the south end of Geer Road.

The elements proposed for the length of the corridor include street trees, (defined in the Street Tree Master Plan), planted medians, brick paving, street signs, and bus shelters. The existing median network should be improved to include additional street trees, modified understory planting, and brick paving. Brick paving introduced as an accent into the sidewalk pavement will enhance the pedestrian experience and reinforce the urban environment associated with the downtown. Bus stops located along the corridor enhanced with new bus shelters and brick paving will unify the corridor, promote ridership, and create a pedestrian-friendly environment. Commercial and business signage consolidated into pedestrian-scale monuments will minimize the negative impact of visual clutter along the corridor.

Directional monumentation, roadway approach signs, crosswalks, banners, planters, seasonal interest plants, and public art incorporated into the identified key intersections will clarify direction and reinforce corridor hierarchy. The design for the key intersections located at Monte Vista Avenue should be emphasized to reflect the campus environment and reinforce a pedestrian atmosphere. The proposed public and private signage should be addressed in greater detail in the Sign Plan.

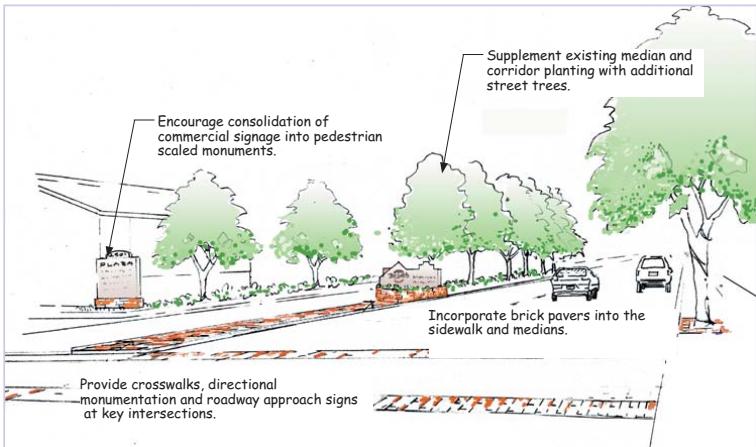
KEY INTERSECTIONS

- Geer Rd. / Christoffersen Parkway
- Geer Rd. / Monte Vista Ave.
- Geer Rd. /Fulkerth Rd.
- Geer Rd. /East Canal Dr.
- Geer Rd. / Golden State Blvd.

GATEWAY ZONES GATEWAY CORRIDORS



Existing



Proposed

C ONCEPTUAL DESIGN SOLUTION

GEER ROAD

The design for the Geer Road corridor should extend the entry experience and promote a pedestrian-friendly environment that reinforces the pedestrian connection to the CSU campus and the downtown. The elements proposed for the corridor should provide an urban environment reflective of Turlock's downtown and should include historic light fixtures, sidewalks enhanced with brick pavers, and public art. Brick planters, enhanced crosswalks, and seasonal interest plants introduced at the identified key intersections will create a pedestrian-friendly environment and promote foot traffic. Enhancement of the existing street tree and median network will serve to unify the corridor and visually buffer pedestrians from vehicular traffic. Consolidation of commercial and business signage into pedestrian-scale monuments will reduce the amount of visual clutter along the corridor.

Goals

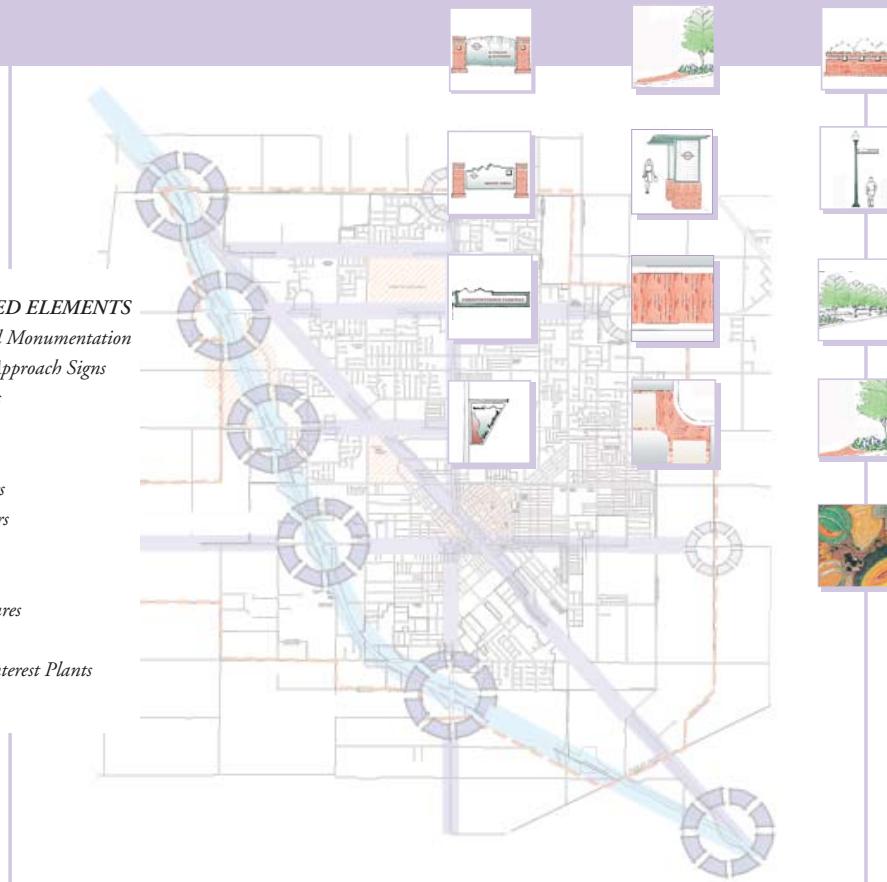
- ❖ Extend the Arrival Experience
- ❖ Reinforce the Identity of Turlock
- ❖ Enhance the Character of the Corridor
- ❖ Clarify Direction
- ❖ Mitigate Negative Visual Impacts

Implementation

- ❖ Incorporate urban, pedestrian-scale elements to include crosswalks, historic light fixtures, brick planters, brick pavers, and public art.
- ❖ Incorporate roadway approach signs and directional monumentation at key intersections.
- ❖ Supplement the existing street tree planting and understory planting in the medians and along the corridor.
- ❖ Implement consolidation of commercial signage.

GATEWAY ZONES GATEWAY CORRIDORS

- PROPOSED ELEMENTS**
- Directional Monumentation
 - Roadway Approach Signs
 - Street Signs
 - Banners
 - Medians
 - Bus Shelters
 - Brick Pavers
 - Crosswalks
 - Planters
 - Light Fixtures
 - Street Trees
 - Seasonal Interest Plants
 - Public Art



LANDER AVENUE

Lander Avenue is an arterial that is classified as a gateway corridor from Clausen Road to West Main. The proposed elements should reinforce the small scale commercial character of the corridor, provide a pedestrian environment, and reinforce the connection to downtown.

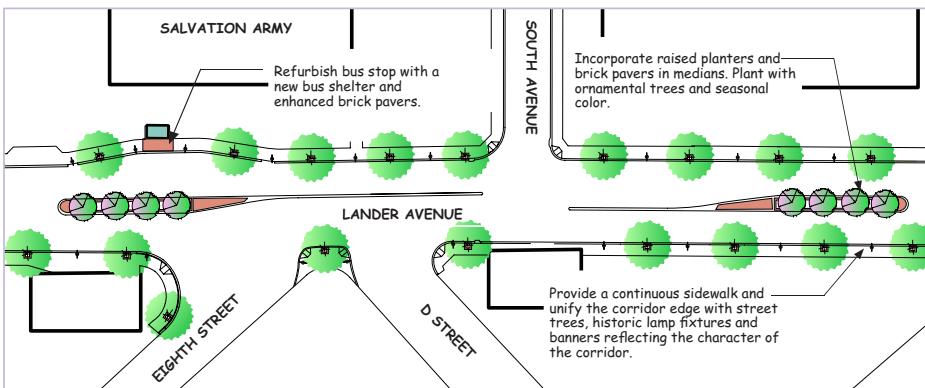
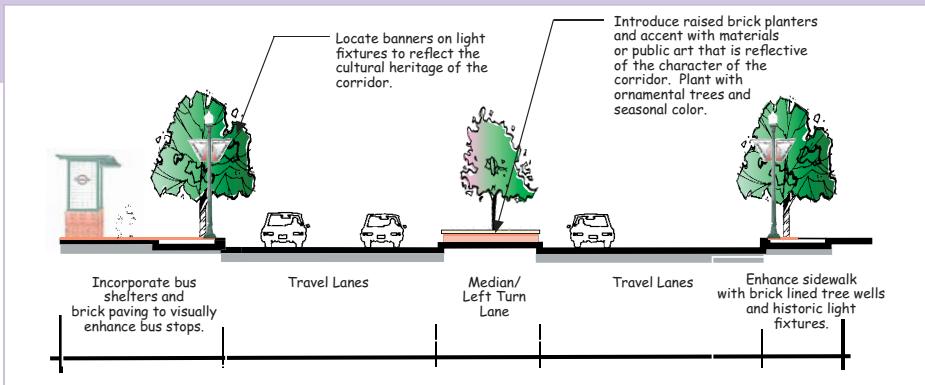
The elements proposed for the length of the corridor include street trees, (defined in the Street Tree Master Plan), planted medians, planters, brick paving, street signs, and bus shelters. The existing median network should be extended and enhanced with the addition of street trees, understory planting, and brick pavers. Brick pavers should also be introduced as accent in the sidewalk pavement and to reinforce the connection to downtown. The addition of planters will establish the commercial nature of the corridor and provide opportunity to introduce materials that reflect the character of the corridor. Strict enforcement of the sign code and consolidation of commercial and business signage into pedestrian-scale monuments will reduce the negative impact of visual clutter along the corridor.

Directional monumentation, roadway approach signs, crosswalks, banners, seasonal interest plants, and public art incorporated into the identified key intersections will clarify direction and reinforce corridor hierarchy. The design and location of the banners and proposed public and private signage should be addressed in greater detail in the Signage Plan.

KEY INTERSECTIONS

Lander Ave. / West Main St.

GATEWAY ZONES GATEWAY CORRIDORS



C ONCEPTUAL DESIGN SOLUTION

LANDER AVENUE

The emphasis of this conceptual design solution is to extend the arrival experience and clarify direction through repetition and visual reinforcement of the design vocabulary introduced in the gateway design. The design for the corridor should create a pedestrian friendly environment, reinforce the small scale commercial character of the corridor, and reflect the connection to the downtown. The existing sidewalk should be extended the length of the corridor and enhanced with street trees and brick tree wells. Incorporation of raised brick planters in the medians planted with seasonal interest plants will unify the corridor and provide an opportunity to incorporate accent materials that reflect the character of the corridor. Banners integrated into the corridor design will add color and excitement and provide a place to publicize special events associated with the corridor and the downtown. Bus stops enhanced with bus shelters and brick pavers in the sidewalk will provide a strong unifying element along the corridor, promote ridership, and reflect a pedestrian friendly environment.

Goals

- ↪ Extend the Arrival Experience
- ↪ Reinforce the Identity of Turlock
- ↪ Enhance the Character of the Corridor
- ↪ Clarify Direction

Implementation

- ↪ Incorporate elements that reinforce the small scale commercial character of the corridor including brick planters, light fixtures, banners, brick paving, and bus shelters.
- ↪ Create a pedestrian friendly environment by providing a continuous sidewalk accented with brick tree wells and street trees.